



DEPARTMENT OF COMMUNITY DEVELOPMENT SERVICES

Planning Division

m e m o r a n d u m

TO: The Urbana Plan Commission

FROM: Jeff Engstrom, AICP, Planner II

DATE: November 15, 2013

SUBJECT: Plan Case No. 2216-SU-13: Request by Southwind RAS LLC for a Special Use Permit to establish a Recycling Center and for a Conditional Use Permit to allow two principal uses on a lot at 910 – 1000 West Saline Court in the IN-2, Heavy Industrial Zoning District

Introduction

Southwind RAS, LLC is proposing to develop an asphalt shingle recycling facility at 910 and 1000 W. Saline Court, in the IN-2, Heavy Industrial Zoning District. The facility will consist of asphalt shingle shredding equipment and storage of materials. The operation will take place primarily on 1000 W. Saline Court, but a part of the operations will be located on an adjacent lot at 910 W. Saline Court. According to Table V-1 of the Urbana Zoning Ordinance, a recycling facility in the IN-2 District requires a Special Use Permit.

Section V-3.C of the Urbana Zoning Ordinance allows for more than one principal use or structure on a single parcel in any zoning district if a Conditional Use Permit is approved. Because 910 W. Saline Court is planned to house a separate construction waste recycling facility, owned by Henson Waste, a Conditional Use Permit is required to allow two uses on this lot. While this conditional use permit is not technically necessary until Henson brings their development application forward, issuing the permit now will ensure that the Southwind facility on the north half of the lot will be allowed to remain on the lot when Henson develops the south part of the lot. The Henson facility will recycle waste materials from construction sites. It will require its own Special Use Permit, which will be applied for early next year, as soon as they have obtained a siting permit from the Illinois Environmental Protection Agency.

The Plan Commission must review the Special Use Permit and Conditional Use Permit application, hold a public hearing, and make a recommendation to the Urbana City Council. The Urbana City Council shall approve, approve with certain conditions, or deny the application.

Background

Description of the Site and Surrounding Properties

The subject property consists of two lots which were recently final platted as North Lincoln Avenue Industrial Park No. 5 (Plan Case 2204-S-13). These lots are currently vacant and are located north and east of West Saline Court, which is at the northwestern edge of the City. The lots created by the plat have addresses of 910 West Saline Court and 1000 West Saline Court. Southwind RAS, LLC proposes to establish a recycling center to recycle asphalt shingles at 1000 West Saline Court, with some of the operation to encroach onto the north 400 feet of 910 West Saline Court. The remainder of 910 West Saline Court is proposed to be developed by Henson Disposal as a construction waste recycling center. Henson is currently seeking Illinois EPA approval for a siting permit and will apply with the City for a Special Use Permit to allow the recycling center once the siting permit is approved in the first half of 2014.

1000 West Saline Court is 2.283 acres in area and 910 West Saline Court is six acres. The surrounding area is industrial and agricultural in nature. To the west (across Saline Court) is an Emulsicoat asphalt facility and the Republic Services waste transfer station. To the north is a parcel owned by Champaign Asphalt Company, currently used for cropping. Immediately east of the site is planned right-of-way for the future Lincoln Avenue realignment, with undeveloped land further to the east. To the south is a vacant parcel and a detention basin, and further south is a concrete recycling facility.

The following chart identifies the current zoning, existing land uses, and Comprehensive Plan future land use designations of the site and surrounding properties.

Direction	Zoning	Existing Land Use	Future Land Use
Site	IN-2, Heavy Industrial	Vacant	Heavy Industrial
North	IN-2, Heavy Industrial	Agriculture	Heavy Industrial
East	County CR, Conservation-Recreation	Vacant	Residential
South	IN-2, Heavy Industrial	Vacant / Industrial	Heavy Industrial
West	IN-2, Heavy Industrial	Industrial	Heavy Industrial

Proposed Use

The proposed use would be an asphalt shingle recycling center. Material in the form of asphalt shingles torn from houses and buildings in the Champaign-Urbana region would be dropped off, then sorted, shredded, screened, and stored on site until it can be hauled away to a road construction site, where it will be mixed in with asphalt for pavement. Exhibit D contains a site plan that shows the proposed layout of the facility. The facility would consist of five designated areas for each stage of the recycling process, with some portable machinery that would be

brought onto the site when it is active. Exhibit E consists of the Engineering Plan for the site, and shows details for the entry drive for the site, as well as storm water drainage facilities and a security fence.

The applicant states that recycled asphalt shingles provide a green technology for paving streets with substantial environmental benefits. Asphalt shingles are the third largest source of construction landfill material each year, so adding this recycling facility will reduce the amount of wasted material in our region. Using recycled asphalt shingles requires less liquid asphalt in road construction and adds to tensile strength and durability of the roadway. The material requires less compaction and allows for faster paving operations. The process for recycling asphalt shingles has been approved by the Illinois EPA and results in re-use of approximately 99% of the asphalt from shingles.

As shown on the site plan, the southern portion of the Southwind site would be paved with asphalt and the northern half would have a surface of Recycled Asphalt Product (R.A.P.). The site plan conforms to applicable development regulations, including required setbacks. A 25-foot front yard setback is required in the IN-2 district. According to Section VI-6.E of the Urbana Zoning Ordinance, materials stored on an industrial parcel must be screened from adjacent right-of-way with a six-foot to eight-foot solid fence or wall and a landscape buffer. The landscape buffer must consist of one tree and three bushes every 40 linear feet. The engineering site plan, Exhibit E, shows a six-foot chain-link security fence around the Southwind site. Staff is recommending that a full landscape plan be submitted and adhered to as a condition of the Special Use Permit to show compliance with the screening and landscape requirements of the Zoning Ordinance. The applicant has agreed to provide required screening and landscaping.

Access to the operation is to be from Saline Court, as the future Lincoln Avenue extension will be designated a minor arterial with limited access points. The Southwind RAS facility will share an access drive with the proposed Henson Waste construction recycling facility, to be located on the southern part of 910 West Saline Court. Exhibit E shows the location of this entrance. The Urbana Public Works Department has commented that this entrance should be connected to the Southwind site with a dust-free, all-weather surface in order to prevent vehicles from spreading soil off the site and into the street. The applicant has updated the site plan to meet this requirement.

Ordinarily, a conditional use permit is required to allow the establishment of more than one use on a single lot per Section V-3.C of the Urbana Zoning Ordinance. However, because a Special Use Permit is required for the recycling operation at this location, it is appropriate to incorporate the Conditional Use permission as a part of the Special Use Permit.

Discussion

Requirements for a Special Use Permit

According to Section VII-4.A of the Urbana Zoning Ordinance, an application for a Special Use Permit shall demonstrate the following:

1. That the proposed use is conducive to the public convenience at that location.

The subject property is located in the North Lincoln Avenue Industrial Park subdivision. This subdivision is developed with heavy industrial uses, including an asphalt plant, concrete recycling facilities, and a waste transfer station. The nearest residence is approximately 1,200 feet to the east, located along the current North Lincoln Avenue. The site has convenient access to Interstate 74 and the future Olympian Drive (via the future North Lincoln Avenue realignment). The Urbana Comprehensive Plan designates this area of the City for Heavy Industrial uses. The proposed Henson construction recycling waste facility on the southern portion of 910 Saline Court will be a compatible use to the Southwind recycling facility because it will accept construction waste, allowing for haulers to bring both types of waste to one facility.

2. That the proposed use is designed, located, and proposed to be operated so that it will not be unreasonably injurious or detrimental to the district in which it shall be located, or otherwise injurious to the public welfare.

The proposed asphalt shingle recycling facility will not be injurious to the public at this location. The applicant states there are no known ground or water contaminants from the process. Dust will be controlled by spraying the materials with water during the recycling process. The proposed special use will allow for a facility that benefits the public in that it will divert materials from local landfills and re-use that material in construction of roads throughout the state. The site is 1,200 feet from the nearest residence. The applicant is seeking approval from Illinois EPA for a beneficial use permit and will conform to all environmental standards. The facility would not be detrimental to the proposed Henson construction waste recycling facility on the southern part of 910 Saline Court.

3. That the proposed use conforms to the applicable regulations and standards of, and preserves the essential character of, the district in which it shall be located, except where such regulations and standards are modified by Section VII-7.

The character of the IN-2, Heavy Industrial District will be preserved with the proposed special use. Setbacks and drainage requirements are met in the attached site plans. The proposed development would conform to applicable regulations for the IN-2, Heavy Industrial District. The applicant has agreed to install required screening and landscaping for industrial uses with outdoor storage. The future Henson construction waste recycling facility will need to meet all applicable regulations when their portion of the site is developed next year.

Requirements for a Conditional Use Permit

In order to approve the multiple uses of asphalt shingle recycling and construction recycling at 910 West Saline Court, a Conditional Use Permit may be granted as part of this Special Use Permit. Since a Special Use Permit requires a higher level of review, City legal staff have indicated that it is appropriate to incorporate the conditional permission of multiple uses on one site into a Special Use Permit. The following requirements apply to allowing the multiple uses of asphalt shingle recycling and construction recycling on a single property. The criteria are similar to that for a Special Use Permit.

According to Section VII-2 of the Urbana Zoning Ordinance, an application for a Conditional Use Permit shall demonstrate the following:

1. *That the proposed use is conducive to the public convenience at that location.*
2. *That the proposed use is designed, located, and proposed to be operated so that it will not be unreasonably injurious or detrimental to the district in which it shall be located, or otherwise injurious to the public welfare.*
3. *That the proposed use conforms to the applicable regulations and standards of, and preserves the essential character of, the district in which it shall be located, except where such regulations and standards are modified by Section VII-3.*

These criteria are essentially the same as those for the Special Use Permit, and have been addressed in the previous section. The proposed uses of asphalt shingle recycling and construction recycling are similar and will fit together well on a single, six-acre parcel. Even though the Henson construction recycling facility is not yet established, granting the Conditional Use Permit for two uses at this time will allow Henson to proceed with the IEPA permitting process and set the table for them to apply for a Special Use Permit once IEPA permits have been obtained.

The Plan Commission shall determine whether the reasons set forth in the application, and the evidence adduced during the public hearing, justify the granting of the Special Use Permit, and whether the proposed use will be in harmony with the general purpose and intent of the Zoning Ordinance, and will not be unreasonably injurious or detrimental to the district in which it shall be located, or otherwise injurious or detrimental to the public welfare.

In addition, the Plan Commission shall make a recommendation to the City Council for or against the proposed special use, and may also recommend such additional conditions and requirements on the operation of the proposed use as are appropriate or necessary for the public health, safety, and welfare, and to carry out the purposes of this Ordinance, including but not limited to conditions that:

1. Regulate the location, extent, and intensity of such use;
2. Require adherence to an approved site plan;
3. Require landscaping and the screening of such use by means of fences, walls, or vegetation;
4. Stipulate a required minimum lot size, minimum yards, and maximum height of buildings and structures;
5. Regulate vehicular access and volume, and the design and location of parking and loading areas and structures;
6. Require conformance to health, safety, and sanitation requirements as necessary;
7. Regulate signs and outdoor lighting;
8. Any other conditions deemed necessary to affect the purposes of the Zoning Ordinance.

Summary of Findings

1. Southwind RAS, LLC is requesting a Special Use Permit to establish an asphalt shingle recycling facility at 910 and 1000 West Saline Court in the IN-2, Heavy Industrial Zoning District.
2. The proposed Special Use Permit would include approval of a Conditional Use Permit to allow the asphalt shingle recycling as an additional principal use at 910 West Saline Court, which is planned to contain a construction waste recycling facility.
3. The proposed use will allow for recycling of asphalt shingles and will divert materials from nearby landfills.
4. The proposed use is conducive to the public convenience at this location, and is located in an area which already contains heavy industrial uses.
5. The proposed use would not be unreasonably injurious or detrimental to the district in which it shall be located.
6. The proposed use meets the regulations and standards of, and preserves the essential character of the IN-2 district and industrial subdivision in which it shall be located.
7. The proposed conditional use permit to allow a second principal use on the site at 910 West Saline Court meets the criteria set forth in the Zoning Ordinance.
8. The proposed use is consistent with the Heavy Industrial designation, as identified in the 2005 Urbana Comprehensive Plan Future Land Use Map.

Options

The Plan Commission has the following options for recommendations to the City Council. In Plan Case 2216-SU-13, the Plan Commission may:

1. Recommend approval of the special use permit without any additional conditions.
2. Recommend approval of the special use permit with any conditions deemed appropriate or necessary for the public health, safety, and welfare, and to carry out the purposes of the Zoning Ordinance
3. Recommend denial of the special use permit.

Recommendation

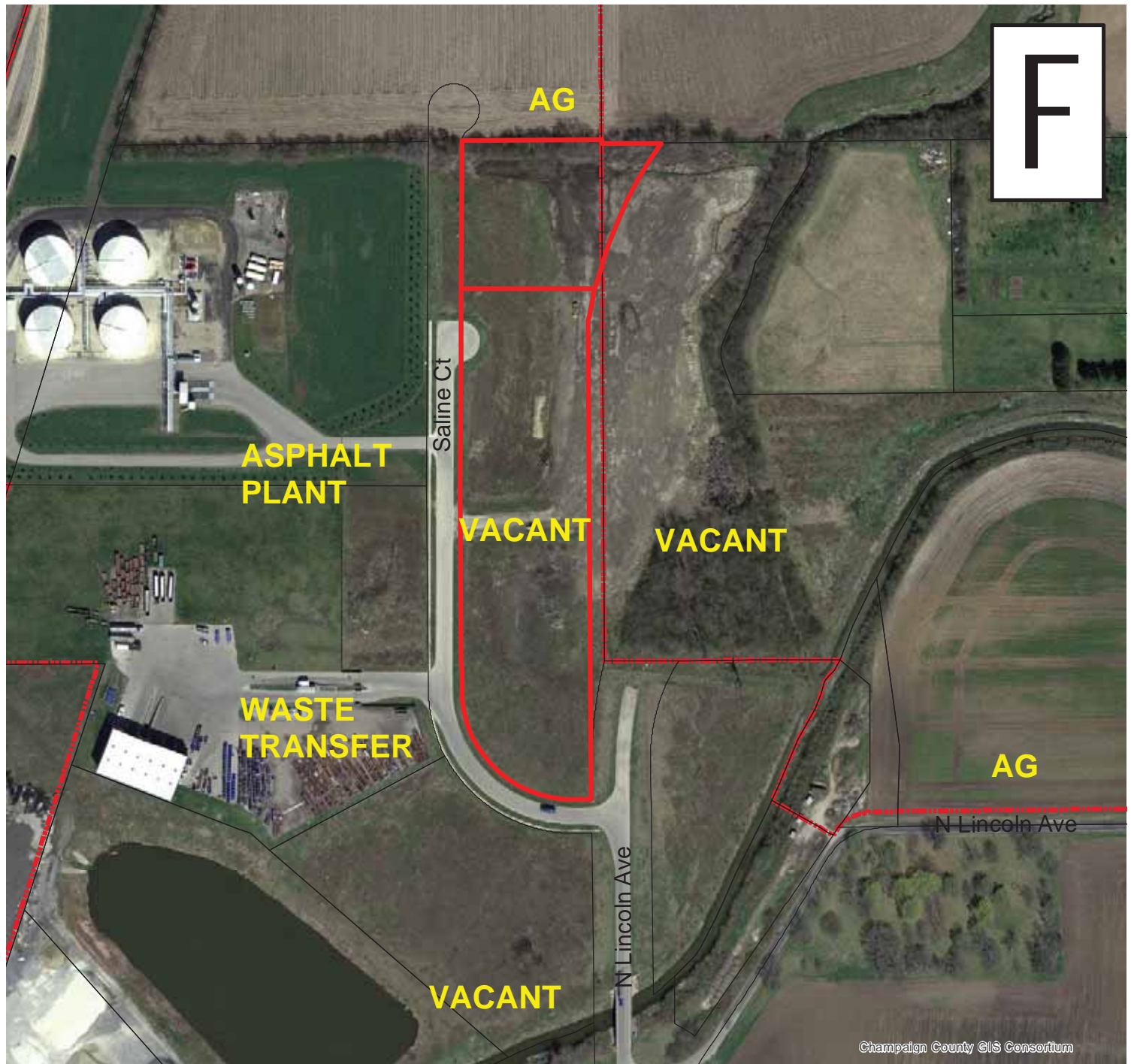
Based on the evidence presented in the discussion above, and without the benefit of considering additional evidence that may be presented at the public hearing, staff recommends that the Plan Commission forward Plan Case No. 2216-SU-13 to the Urbana City Council with a recommendation for APPROVAL with the following CONDITIONS:

1. That the development shall be constructed in general conformance with the attached site plan. The Zoning Administrator shall have the authority to approve minor changes substantially in compliance with the approved site plan necessary for the project to comply with City regulations including building, fire, and site development codes.
2. That a landscape plan is submitted, reflecting conformance with Zoning Ordinance standards for screening of industrial storage and required landscape buffers.
3. That all required Illinois EPA permits be obtained prior to development of the site.
4. That the conditional use permit to allow two uses at 910 West Saline Court shall only be valid if a separate Special Use Permit is issued to allow the Henson construction waste facility.

Attachments: Exhibit A: Location and Existing Land Use Map
 Exhibit B: Existing Zoning Map
 Exhibit C: Future Land Use Map
 Exhibit D: Application for a Special Use Permit with Conceptual Site Plan
 Exhibit E: Engineering Site Plans

CC: Southwind RAS, LLC

EXHIBIT A: LOCATION & LAND USE MAP



0 90 180 360 540 720
Feet



Case: 2216-SU-13
Subject: Application for Special Use Permit to establish Recycling Facility and Conditional Use Permit for two principal uses on one lot
Location: 910 - 1000 W. Saline Ct
Petitioner: Southwind RAS, LLC



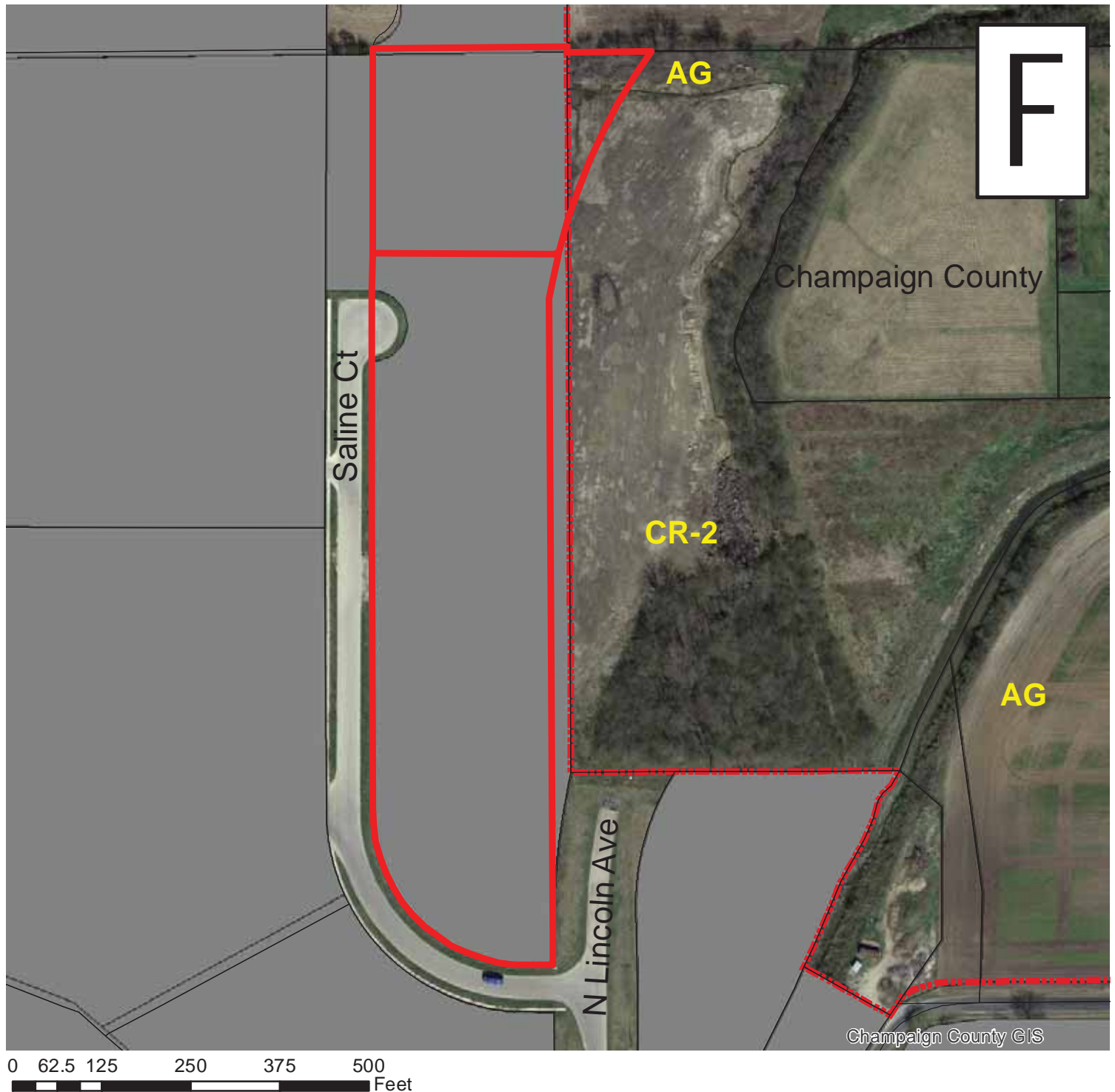
-  Subject Property
-  Urbana Corporate Limits

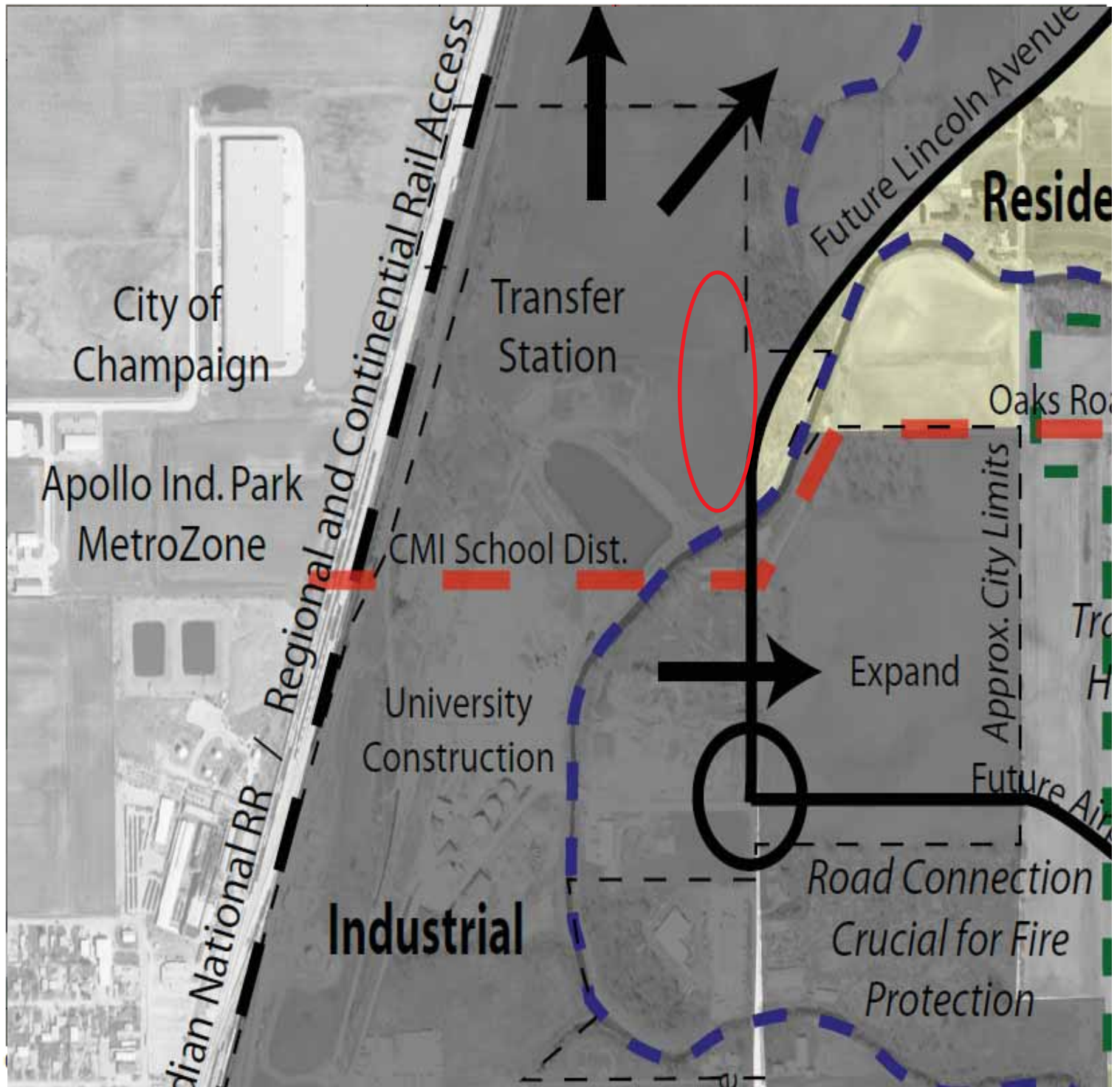
EXHIBIT B: EXISTING ZONING MAP



Case: 2216-SU-13
Subject: Application for Special Use Permit to establish Recycling Facility and Conditional Use Permit for two principal uses on one lot
Location: 910 - 1000 W. Saline Ct
Petitioner: Southwind RAS, LLC

- Subject Property
- Urbana Corporate Limits
- IN-2

EXHIBIT C: FUTURE LAND USE MAP



Case: 2216-SU-13
 Subject: Application for Special Use Permit to establish Recycling Facility and Conditional Use Permit for two principal uses on one lot
 Location: 910 - 1000 W. Saline Ct
 Petitioner: Southwind RAS, LLC

 Subject property (approximate)



Application for Special Use Permit

Plan Commission

APPLICATION FEE - \$175.00

The Applicants are responsible for paying the cost of legal publication fees as well. The fees usually run from \$75.00 to \$125.00. The applicant is billed separately by the News-Gazette.

DO NOT WRITE IN THIS SPACE - FOR OFFICE USE ONLY

Date Request Filed 10-29-2013 Plan Case No. 2216-SU-13
 Fee Paid - Check No. 4595 Amount \$175.00 Date 10-28-2013

PLEASE PRINT OR TYPE THE FOLLOWING INFORMATION

A SPECIAL USE PERMIT is requested in conformity with the powers vested in the Plan Commission to recommend to the City Council under Section IN-2 HEAVY INDUSTRIAL DISTRICT UNDER TABLE V-1 of the Urbana Zoning Ordinance to allow *(Insert proposed use)* RECYCLING FACILITY FOR RECYCLING OF ASPHALT SHINGLES (RAS) PERSUANT TO A SECTION 22.38 & 22.54 PERMIT FROM THE IEPA on the property described below.

1. APPLICANT CONTACT INFORMATION

Name of Applicant(s): **SOUTHWIND RAS, LLC** Phone: **630-698-4700**
 Address (street/city/state/zip code): **2250 SOUTHWIND BLVD., BARTLETT, IL 60103**
 Email Address: **RICH@WYDP.COM**

2. PROPERTY INFORMATION

Address/Location of Subject Site: **1000 W. SALINE COURT, URBANA, IL. LOT 502 & 501 OF NORTH LINCOLN AVE. INDUSTRIAL PARK SUBDIVISION NO. 5**
 PIN # of Location: **91-15-31-200-006**
 Lot Size: LOT 501 (6 ACRES) & LOT 502 (2.283) ACRES

Current Zoning Designation: **IN-2 HEAVY INDUSTRIAL**

Current Land Use (vacant, residence, grocery, factory, etc): **VACANT**

Proposed Land Use: **RECYCLING FACILITY FOR RECYCLING OF ASPHALT SHINGLES**

(RAS) PERSUANT TO A SECTION 22.38 PERMIT FROM THE IEPA

Legal Description: Lots 501 and 502 in the North Lincoln Avenue Industrial Park Subdivison No., 5, part of the NE 1/4 of Sec. 31, and part of the NW 1/4 of sec. 32, T.20N., R.9E., 3rd P.M. in the City of Urbana, Champaign County, Illinois.

3. CONSULTANT INFORMATION

Name of Architect(s): N/A

Phone:

Address (*street/city/state/zip code*):

Email Address:

Name of Engineers(s): TOM JORDAN, FOTH INFRASTRUCTURE & ENV. Phone: 217-353-7322

Address (*street/city/state/zip code*): 1610 BROADMOOR, CHAMPAIGN, IL 61821

Email Address: TOM.JORDAN@FOTH.COM

Name of Surveyor(s): TOM JORDAN, FOTH

Phone: 217-353-7322

Address (*street/city/state/zip code*): 1610 BROADMOOR, CHAMPAIGN, IL 61821

Email Address: TOM.JORDAN@FOTH.COM

Name of Professional Site Planner(s): N/A

Phone:

Address (*street/city/state/zip code*):

Email Address:

Name of Attorney(s): RICHARD GUERARD

Phone: 630-698-4700

Address (*street/city/state/zip code*): 310 S. COUNTY FARM ROAD, WHEATON, IL

Email Address: RICH@WYDP.COM

4. REASONS FOR SPECIAL USE PERMIT

Explain how the proposed use is conducive to the public convenience at the location of the property.

THE LOCATION IS WITHIN THE NORTH LINCOLN AVE INDUSTRIAL PARK SUBDIVISON NO. 5, WITH GOOD ACCESS TO PUBLIC HIGHWAYS AND ZONED IN-2 HEAVY INDUSTRIAL WHICH IS THE APPROPRIATE ZONING FOR THE PROPOSED USE. THE USE IS COMPATIBLE WITH EXISTING INDUSTRIAL USES IN THE GENERAL AREA (SOLID WASTE TRANSFER STATION, OIL STORAGE FACILITY,

ASPHALT PLANT AND CONCRETE RECYCLING FACILITY).

Explain how the proposed use is designed, located and proposed to be operated, so that it will not be unreasonably injurious or detrimental to the district in which it shall be located, or otherwise injurious or detrimental to the public welfare.

THE PROPERTY IS IN AN INDUSTRIAL PARK ZONED IN-2 HEAVY INDUSTRIAL. RAS IS AN ACRONYM FOR "RECYCLED ASPHALT SHINGLES" WHICH ARE "CHOPPED", SCREENED FOR SIZE AND THEN SENT TO AN ASPHALT PLANT AND USED TO PRODUCE A QUALITY ASPHALT PAVING MATERIAL. THE MATERIALS ARE NOT ENVIRONMENTALLY DIFFERENT FROM THE ROOFS, DRIVEWAYS AND COUNTY AND VILLAGE STREETS, FROM THE THOUSANDS OF HOMES WHICH USE THESE MATERIALS. THEY ARE THE SAME MATERIALS.

THERE ARE NO KNOWN SOURCES OF GROUND AND WATER POLLUTION. DUST CAN BE CREATED WITHIN THE EQUIPMENT BY THE GRINDING OPERATION AND IS CONTROLLED BY WATER SPRAY BARS WITHIN THE EQUIPMENT. THERE ARE NO CHEMICALS, HEAT, SMOKE OR ODOR FROM THE PROCESS. PORTABLE EQUIPMENT AND TRUCKS ARE THE ONLY NOISE AND WILL BE BELOW THE ZONING STANDARDS.

Explain how the proposed use conforms to the applicable regulations and standards of and preserves the essential character of the district in which it shall be located.

THE PROPERTY IS LOCATED IN AN INDUSTRIAL PARK ZONED IN-2 HEAVY INDUSTRIAL. THE PARK HAS BEEN PLANNED FOR SUCH USES. THERE ARE NO INCOMPATIBLE USES NOW EXISTING IN THE INDUSTRIAL PARK. RECYCLING ASPHALT SHINGLES (RAS) PROVIDES A GREEN TECHNOLOGY FOR PAVING STREETS. THERE ARE SUBSTANTIAL ENVIRONMENTAL BENEFITS TO RECYCLING ASPHALT SHINGLES. ASPHALT SHINGLES ARE THE THIRD LARGEST SOURCE OF CONSTRUCTION LAND FILL MATERIAL EACH YEAR. RAS MIX REQUIRES LESS LIQUID ASPHALT, THE MOST EXPENSIVE COMPONENT IN HOT

MIX ASPHALT; ADDS TO THE TENSILE STRENGTH AND DURABILITY OF THE PAVEMENT AND REQUIRES LESS COMPACTION ALLOWING FASTER PAVING. THE IEPA APPROVED PROCESS OF SORTING, TESTING, GRINDING AND SCREENING SHINGLES RESULTS IN THE RE-USE OF APPROXIMATELY 99% OF ALL MATERIALS IN THE RECYCLING PROCESS. RAS IS APPROVED FOR USE IN PAVING BY IDOT, THE TOLLWAY AUTHORITY, THE CITY OF CHICAGO AND MANY OTHER COUNTIES AND MUNICIPALITIES IN ILLINOIS.

THE RAS USE WILL NOT BE HAZARDOUS OR DISTURBING TO EXISTING OR FUTURE NEIGHBORHOOD USES. THE USE WILL NOT CAUSE SUBSTANTIAL INJURY TO THE VALUE OF OTHER PROPERTY IN THE NEIGHBORHOOD IN WHICH IT IS LOCATED AND WILL COMPLY WITH ALL LOCAL, STATE AND FEDERAL PERFORMANCE STANDARDS AND CITY OF URBANA ORDINANCES, CODES AND REQUIREMENTS.

NOTE: If additional space is needed to accurately answer any question, please attach extra pages to the application.

By submitting this application, you are granting permission for City staff to post on the property a temporary yard sign announcing the public hearing to be held for your request.

CERTIFICATION BY THE APPLICANT

I certify all the information contained in this application form or any attachment(s), document(s) or plan(s) submitted herewith are true to the best of my knowledge and belief, and that I am either the property owner or authorized to make this application on the owner's behalf.



Applicant's Signature

10.28.2013
Date

PLEASE RETURN THIS FORM ONCE COMPLETED TO:

City of Urbana
Community Development Department Services
Application for Special Use Permit – Updated June, 2012

A. GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 01, 2012", THESE PLANS, THE FOLLOWING TECHNICAL SPECIAL PROVISIONS AND AS AMENDED HEREIN.
- CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES BEFORE BEGINNING CONSTRUCTION. CONTRACTOR SHALL CALL J.U.L.I.E. PRIOR TO BEGINNING CONSTRUCTION.
- ALL EXCESS MATERIAL SHALL BE DISPOSED OF OFF THE JOB SITE BY THE CONTRACTOR OR AS APPROVED BY THE OWNER.
- THE THICKNESS OF EXISTING HMA SURFACE SHOWN ON THE PLANS REPRESENTS THE ENGINEER'S BEST ESTIMATE OF THE AVERAGE THICKNESS. ACTUAL THICKNESS WILL VARY MORE THAN OR LESS THAN THE SPECIFIED THICKNESS THROUGHOUT THE PROJECT.
- THE CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION LAYOUT FOR THIS PROJECT. THE ENGINEER WILL PROVIDE THE CONTRACTOR WITH AN AUTOCAD V. 2008 MODEL DRAWING OF THE SITE IMPROVEMENTS SHOWN ON THESE PLANS FOR HIS USE FOR CONSTRUCTION LAYOUT AND STAKING BY TOTAL STATION. ADEQUATE PRIMARY CONTROL EXISTS ON-SITE FOR THE CONTRACTOR TO ESTABLISH HIS HORIZONTAL POSITION ON THE SITE.

UNDERGROUND UTILITY NOTE:
UNDERGROUND UTILITIES SHOWN HEREIN ARE PLOTTED FROM INFORMATION PROVIDED BY OTHERS. THE CONTRACTOR SHALL USE EXTREME CAUTION AND LOCATE ALL UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION ACTIVITIES.

B. GENERAL NOTES - AUTHORITY AND RESPONSIBILITY

- THE ENGINEER SHALL NOT BE RESPONSIBLE FOR THE MEANS, METHODS, PROCEDURES, TECHNIQUES, OR SEQUENCES OF CONSTRUCTION, NOR SAFETY ON THE JOB SITE, NOR SHALL THE ENGINEER BE RESPONSIBLE FOR THE CONTRACTOR'S FAILURE TO CARRY OUT THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. NEITHER THE PROFESSIONAL ACTIVITIES OF THE ENGINEER NOR THE PRESENCE OF THE ENGINEER AT A CONSTRUCTION SITE SHALL RELIEVE THE CONTRACTOR OF THEIR OBLIGATIONS, DUTIES, AND RESPONSIBILITIES INCLUDING ANY HEALTH AND SAFETY PRECAUTIONS REQUIRED BY ANY REGULATORY AGENCIES.

IN ADDITION, THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SHALL BE MODIFIED AS FOLLOWS:

UNDER SECTION 105, ADD THE FOLLOWING SENTENCE:

NOTHING CONTAINED HEREIN SHALL RELIEVE CONTRACTOR OF ITS DUTY TO OBSERVE AND COMPLY WITH ALL APPLICABLE LAWS, NOR SHALL ENGINEER BE RESPONSIBLE FOR CONTRACTOR'S COMPLIANCE OR NON-COMPLIANCE WITH SUCH LAWS.

UNDER SECTION 107.01, ADD THE FOLLOWING SENTENCE:

ENGINEER SHALL NOT BE RESPONSIBLE FOR CONTRACTOR'S DUTY TO OBSERVE AND COMPLY WITH THE PROVISIONS OF THIS SECTION, OR FOR CONTRACTOR'S FAILURE TO DO SO.

C. TECHNICAL SPECIAL PROVISIONS

- THE FOLLOWING SPECIAL PROVISIONS SUPPLEMENT THE STANDARD SPECIFICATIONS. WHEREIN THESE SPECIAL PROVISIONS, OR IN THE PLANS, THE TERM "STANDARD SPECIFICATIONS" IS REFERRED TO, IT SHALL BE UNDERSTOOD TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION JANUARY 1, 2012; THE LATEST EDITION OF THE ILLINOIS "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", IN EFFECT ON THE DATE OF BIDS; AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2013, PORTIONS INCLUDED HEREIN WHICH APPLY TO AND GOVERN THE CONSTRUCTION OF PUBLIC IMPROVEMENTS, AND IN CASE OF CONFLICT WITH ANY PART, OR PARTS, OF SAID SPECIFICATIONS, THESE TECHNICAL SPECIAL PROVISIONS SHALL TAKE PRECEDENCE AND SHALL GOVERN.

1. TRAFFIC CONTROL AND PROTECTION

- THE CONTRACTOR'S CONSTRUCTION OPERATIONS SHALL BE CONFINED WITHIN THE CONSTRUCTION LIMITS. THE CONSTRUCTION LIMITS SHALL BE DEFINED GENERALLY AS THE CONSTRUCTION IMPROVEMENT AREA SHOWN ON THE PLANS. IF THE CONTRACTOR HAS A QUESTION CONCERNING HIS LATERAL LIMITS OF OCCUPANCY DURING CONSTRUCTION, HE SHALL BRING THIS MATTER TO THE ATTENTION OF THE OWNER, DURING THE BIDDING PROCESS FOR CLARIFICATION PRIOR TO SUBMITTING HIS PRICE PROPOSAL TO DO THE WORK.
- ANY STAGING, CONTRACTOR VEHICLE PARKING OR STOCKPILING OF MATERIALS SHALL BE WITHIN THE CONSTRUCTION LIMITS.

2. GRADING AND EROSION CONTROL

- EARTHWORK SHALL BE IN ACCORDANCE WITH THE PLANS, SPECIAL PROVISIONS AND "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" IN ILLINOIS, ADOPTED JANUARY 1, 2012.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR GRADING INCLUDING EXCAVATION, EMBANKMENT AND BACKFILLING AS NECESSARY TO CONSTRUCT ALL PAVEMENT AND SIDEWALKS TO THE ELEVATIONS SHOWN ON THE PLANS AND AS OUTLINED IN THE SPECIAL PROVISIONS. ALL BACKFILLING AND FINAL TOPSOIL PLACEMENT AS SHOWN ON THE PLANS SHALL BE COMPLETED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY DAMAGE DONE TO STORM OR SANITARY MANHOLES OR OTHER UTILITIES DURING GRADING.
- EXISTING TOPSOIL SHALL BE REMOVED AS SHOWN ON THE PLANS AND AS APPROVED BY THE OWNER. REMOVAL SHALL BE TO A MINIMUM DEPTH OF 6 INCHES OR ADDITIONAL DEPTH AS NEEDED TO REMOVE ALL ORGANIC MATERIAL.
- THE ENTIRE SUBGRADE SHALL BE COMPACTED TO NOT LESS THAN 95% OF THE STANDARD LABORATORY DENSITY IN ACCORDANCE WITH ARTICLES 301.03 AND 205.06 OF THE STANDARD SPECIFICATIONS. PRIOR TO COMMENCEMENT OF PAVEMENT CONSTRUCTION, THE IN-SITU SUBGRADE SHALL HAVE A CALIFORNIA BEARING RATE (CBR) OF AT LEAST 6% IN THE TOP 12" OF SUBGRADE AS DETERMINED BY THE TESTING SERVICE USING THE PROCEDURE DESCRIBED IN SUBGRADE COMPACTION VERIFICATION, SEE BELOW.
- CONSTRUCT PAVING SUBGRADE WITH SLOPE AS SHOWN ON THE TYPICAL SECTIONS.
- SOME AREAS OF SUBGRADE MAY REQUIRE STABILIZATION PRIOR TO PLACEMENT OF PAVEMENT. SUBGRADE STABILIZATION, WILL BE ACCOMPLISHED BY REMEDIATION CONSISTING OF 12" CRUSHED STONE (CA-6 OR CA-10) ON GEOTEXTILE FABRIC. ALTERNATE METHOD WILL BE USED ONLY AS APPROVED BY THE OWNER.
- AREAS SHOWN ON THE PLANS SHALL RECEIVE A MINIMUM OF 6 INCHES OF TOPSOIL IN EMBANKMENT AREAS. A MINIMUM OF 6 INCHES OF TOPSOIL SHALL BE PLACED IN EXCAVATION AREAS. EARTH AREAS IN CUT SHALL BE UNDERCUT TO ACCOMMODATE 6" OF TOPSOIL.
- PERMANENT SEEDING SHALL TAKE PLACE BETWEEN THE DATES OF APRIL 1 AND JUNE 15 OR BETWEEN THE DATES OF AUGUST 15 AND SEPTEMBER 30, UNLESS APPROVED OTHERWISE BY THE OWNER.
- ALL AREAS TO BE PERMANENTLY SEEDED SHALL USE CLASS 1 (MODIFIED) OR CLASS 7 (MODIFIED) SEED MIXTURE OR EQUAL IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLES 250.05 AND 250.06 OF THE STANDARD SPECIFICATIONS. HYDRAULIC MULCH, METHOD 3 OF ARTICLE 251 OF THE STANDARD SPECIFICATION SHALL BE USED FOR CLASS 1 (MODIFIED) SEEDING. STRAW MULCH SHALL BE APPLIED TO ALL SEEDED AREAS ACCORDING TO METHOD 2, PROCEDURE 2 OF ARTICLE 251.03 OF THE SPECIFICATIONS FOR CLASS 7 (MODIFIED) SEEDING. FERTILIZER AND LIMESTONE WILL NOT BE APPLIED.
- MODIFICATIONS TO IDOT SEED MIXTURES:

CLASS 1 (MODIFIED)	
PERENNIAL RYE (MANHATTAN, PENFINE)	70#/AC
KENTUCKY BLUEGRASS	100#/AC
CLASS 7 (MODIFIED)	
PERENNIAL RYE (MANHATTAN, PENFINE)	70#/AC
OATS, SPRING	50#/AC

3. PAVING AND STORM SEWERS

- PAVING AND STORM SEWERS SHALL BE CONSTRUCTED IN ACCORDANCE WITH PLANS, SPECIAL PROVISIONS, "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" IN ILLINOIS, ADOPTED JANUARY 1, 2012.
- CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES BEFORE BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER (PH. 352-4169) AT LEAST 48 HOURS IN ADVANCE OF THE START OR RESTART OF CONSTRUCTION.
- SEE SHEET C-3 FOR PAVEMENT DETAILS.
- PROVIDE CONTRACTION JOINTS AT 15' CTRS. MAXIMUM IN P.C.C. PAVEMENT AND CURB AND GUTTER.
- ALL PIPE SHALL BE BEDDED IN 6" SAND OR GRAVEL BEDDING. BEDDING MATERIAL SHALL EXTEND TO UNDISTURBED EARTH. BEDDING SHALL BE PLACED UP HALF THE DIAMETER OF THE PIPE. INITIAL BACKFILL SHALL BE PLACED TO 1.0' OVER THE TOP OF THE PIPE FOR FLEXIBLE PIPE.
- ALL EXCESS MATERIAL SHALL BE DISPOSED OF OFF THE JOB SITE. ALL TRENCHING OPERATIONS WITHIN A PREPARED SUBGRADE SHALL BE BACKFILLED IN SUCH A MANNER THAT THE SUBGRADE WILL REMAIN IN THE CONDITION EXISTING BEFORE CONSTRUCTION OPERATIONS BEGAN. THE CONTRACTOR WILL BE HELD LIABLE FOR THE COST OF ANY GRADING OPERATIONS NECESSARY TO CORRECT FAULTY BACKFILLING OR CLEANUP PROCEDURES.

- ACCEPTABLE MATERIAL FOR STORM SEWERS: WHERE A SPECIFIC MATERIAL IS CALLED FOR IN THE PLANS, THAT MATERIAL CALLED FOR SHALL BE USED AT THAT LOCATION. IF NO MATERIAL IS CALLED FOR, THE FOLLOWING SHALL APPLY.
 - HDPE SEWER PIPE - CORRUGATED POLYETHYLENE (PE) PIPE WITH A SMOOTH INTERIOR CONFORMING TO ARTICLE 1040.04 OF THE STANDARD SPECIFICATIONS. HDPE PIPE, ADS N-129 PROLINK ULTRA, OR EQUAL SHALL BE PROVIDED WITH AN INTEGRAL BELL AND GASKET.
 - PVC - POLYVINYL CHLORIDE (PVC) SEWER PIPE CONFORMING TO ASTM D-3034. MINIMUM WALL THICKNESS SHALL BE STANDARD DIMENSION RATIO (SDR) 26. PIPE JOINTS FOR PVC PIPE SHALL BE FLEXIBLE ELASTOMERIC SEALS CONFORMING TO ASTM D-3212.
 - RCCP - IN ACCORDANCE WITH ARTICLE 1042.06 OF THE STANDARD SPECIFICATIONS, UNLESS OTHERWISE INDICATED. (12" DIAMETER AND LARGER)
 - SUBSTITUTIONS FOR CASTINGS INDICATED ON THE PLANS MUST BE APPROVED IN ADVANCE BY THE ENGINEER.
 - RIM ELEVATIONS ON CURB CASTINGS ARE TO EDGE OF PAVEMENT.
 - ALL STORM SEWER CASTINGS SHALL HAVE THE CAPTION "NO DUMPING - DRAINS TO RIVER" CAST INTO THE SURFACE.
 - TRENCH BACKFILL SHALL BE PROVIDED WHEN THE INNER EDGE OF TRENCH OR MANHOLE EXCAVATION IS WITHIN 2' OF ANY PROPOSED OR EXISTING PAVEMENT, CURB, OR SIDEWALK. NO EXTRA COMPENSATION WILL BE ALLOWED FOR COORDINATION OF THIS WORK.
 - TRENCH BACKFILL COMPACTION BY WATER INUNDATION AND WATER JETTING WILL NOT BE ALLOWED UNDER OR WITHIN 2' OF ANY PUBLIC STREET PAVEMENT OR SIDEWALK.
- ### 4. STORM WATER POLLUTION PREVENTION
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMPLEMENTATION OF THE STORM WATER POLLUTION PREVENTION PLAN INCLUDING THE INSTALLATION AND MAINTENANCE OF EROSION PROTECTION DEVICES, THE INSPECTION OF THE SITE WEEKLY AND WITHIN 24 HOURS OF THE END OF ANY RAIN EVENT OF EQUAL TO OR GREATER THAN 1/2 INCH, AND OTHER ACTIONS NECESSARY TO PREVENT POLLUTION DURING CONSTRUCTION.
 - THE OWNER WILL PROVIDE THE CONTRACTOR WITH A COPY OF THE STORMWATER POLLUTION PREVENTION PLAN, A COPY OF THE NPDES PERMIT, AND IEPA SEWER AND WATER MAIN PERMITS REQUIRED FOR CONSTRUCTION OF THE IMPROVEMENTS.
 - THE CONTRACTOR SHALL KEEP A COPY THE STORM WATER POLLUTION PREVENTION PLAN, EXECUTED COPIES OF THE CONTRACTOR CERTIFICATION STATEMENT (IDOT BDE FORM BC 2342A), EXECUTED COPIES OF THE CONTRACTOR CERTIFICATION STATEMENT (IDOT BDE FORM BC 2342A) FOR ALL SUBCONTRACTORS, COMPLETED WEEKLY AND RAIN EVENT INSPECTION REPORTS (IDOT BDE FORM BC 2259), AND ANY COMPLETED STATEMENTS OF NON-COMPLIANCE (WPC 621) ON SITE, AT ALL TIMES.
 - EROSION PROTECTION SHALL BE INSTALLED IN ACCORDANCE WITH THE APPROPRIATE ARTICLES OF THE ILLINOIS URBAN MANUAL, THE SECTION 280 OF THE STANDARD SPECIFICATIONS, THE CITY OF CHAMPAIGN MANUAL OF PRACTICE, AND THE STORM WATER POLLUTION PREVENTION PLAN. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICES FOR EROSION PROTECTION, WHICH WORK SHALL INCLUDE THE MAINTENANCE OF SAID EROSION PROTECTION.
 - ALL INLETS SHALL BE PROTECTED WITH A PREFABRICATED PROTECTION DEVICE APPROVED BY THE ENGINEER UNTIL PAVEMENT HAS BEEN CONSTRUCTED OR VEGETATION HAS BEEN ESTABLISHED.
 - THE CONTRACTOR SHALL CONTROL TRACKING OF DIRT ON TO PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING DIRT TRACKED ON TO PAVEMENT. THE CONTRACTOR SHALL CONTROL DUST ON THE SITE USING AN APPROPRIATE METHOD APPROVED BY THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE COST OF EARTH EXCAVATION.
 - SILT FENCE AND/OR STONE DITCH CHECKS MAY BE REQUIRED FOR EROSION CONTROL AND SHALL BE PROVIDED AS DESIGNATED BY THE ENGINEER AND CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2012.
- ### 5. SUBGRADE COMPACTION VERIFICATION
- DESCRIPTION - SUBGRADE COMPACTION AND STABILITY REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 301.04 OF THE STANDARD SPECIFICATIONS. SUBGRADE COMPACTION VERIFICATION SHALL BE ASCERTAINED BY A TESTING SERVICE EMPLOYED BY THE CONTRACTOR.
 - THE PAVEMENT SUBGRADE SHALL HAVE SUFFICIENT STABILITY TO ACCOMMODATE CONSTRUCTION TRAFFIC WITHOUT EXCESSIVE SUBGRADE RUTTING OR SHOWING. AT THE TIME OF PLACEMENT OF THE PAVEMENT, THE IN-SITU SUBGRADE SHALL HAVE A CALIFORNIA BEARING RATIO (CBR) OF AT LEAST 6 PERCENT IN THE TOP 12 INCHES OF SUBGRADE. THE CBR PERCENTAGE WILL BE ASCERTAINED BY THE CONTRACTOR BY USE OF THE DYNAMIC CONE PENETROMETER (DCP).

DESCRIPTION OF THE DCP TEST SHALL BE ASCERTAINED BY THE CONTRACTOR BY USE OF THE DYNAMIC CONE PENETROMETER (DCP).

- THE DCP TEST SHALL ALSO BE CONDUCTED TO A DEPTH OF AT LEAST 3 FEET BELOW THE FINISHED GRADE OF THE SUBGRADE TO PROVIDE THE OWNER WITH KNOWLEDGE OF THE UNDERLYING SOIL CONDITIONS.
- THE DCP USED FOR TESTING SHALL BE 17.6 LB. MASS RAISED A DISTANCE OF 22.6 INCHES AND DROPPED, DRIVING A CONE-TIPPED ROD THROUGH THE SOIL. THE DIAMETER OF THE CONE SHALL BE 0.79 INCHES WITH A 60 DEGREE ANGLED TIP. THE RATE OF PENETRATION SHALL BE MEASURED AND CORRELATED TO CBR USING THE RELATIONSHIP ESTABLISHED BY IDOT'S ATTACHMENT 1 OF THE "SUBGRADE CHARACTERIZATION AND SUBGRADE STABILITY", DATED OCTOBER 13, 1992 [LOG CBR = 0.84 - 1.26 * LOG (PR)], PR = IN/BLOW. ANY AREAS OF THE SUBGRADE WHICH FAIL SHALL BE RECONSTRUCTED AND RETESTED WITHOUT ADDITIONAL COMPENSATION.
- EMBANKMENT WITHIN THE STREET SUBGRADE SHALL BE COMPACTED TO NOT LESS THAN 95 PERCENT OF THE STANDARD LABORATORY DENSITY IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS.
- WHEN DIRECTED BY THE OWNER, THE SUBGRADE SHALL ALSO BE TEST ROLLED AND APPROVED IN ACCORDANCE WITH THE FOLLOWING PROCEDURE. TRUCKS SHALL BE LOADED AS FOLLOWS: 27,000 LBS. ON TWO AXLES AND 45,000 LBS. ON THREE AXLES WITH THE TOLERANCE NOT TO EXCEED TEN PERCENT. THE TRUCK SHALL MAKE PARALLEL PASSES ALONG THE SUBGRADE AT DISTANCES AS DIRECTED BY THE ENGINEER AND NOT TO EXCEED TEN FEET APART. ANY AREAS WHICH SHOW RUTTING, CRACKING, OR ROLLING OF THE COMPACTED SUBGRADE UPON TEST ROLLING WILL NOT BE ACCEPTED. THE AREAS THAT FAIL SHALL BE RECONSTRUCTED AND TEST ROLLED AGAIN PRIOR TO ACCEPTANCE.

6. SECURITY FENCE

- 6 FOOT CHAIN LINK FENCE IN ACCORDANCE WITH SECTION 664 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND I.D.O.T. STANDARD 664001. SHOP DRAWINGS REQUIRED.
- GATE SHALL BE 24', TYPE AS APPROVED BY THE OWNER. SHOP DRAWINGS REQUIRED.

EARTHWORK SUMMARY	
EARTH EXCAVATION	5,407 C.Y.
EMBANKMENT	3,707 C.Y.
TOPSOIL FURNISH & PLACE	453 C.Y.
NOTES:	
1. SURPLUS EXCAVATION TO BE PLACED IN SOUTH END OF SITE AS DIRECTED BY THE OWNER.	
2. NO ALLOWANCE MADE TO QUANTITIES SHOWN HEREIN FOR THE R.A.P. SURFACE AT THE SOUTH TWO MATERIAL STORAGE AREAS.	

JOHN A. DABROWSKI
PROJECT DESIGN ENGINEER
IL. PROFESSIONAL
ENGINEER NO. 42530
EXPIRES NOVEMBER 30, 2014

DATE

SUBMITTED FOR
CITY OF URBANA REVIEW
SEPTEMBER 19, 2013

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SOUTHWIND RAS

PHASE 1 RECYCLED ASPHALT SHINGLES

CONTRACT #

CITY OF CHAMPAIGN, ILLINOIS

CHAMPAIGN COUNTY

NO.	BY	DATE	DESCRIPTION	RECORD DRAWING OF COMPLETED CONSTRUCTION BY	RECORD DRAWINGS OF COMPLETED CONSTRUCTION CONFORMING TO CONTRACTOR AND/OR OWNERS RECORDS.

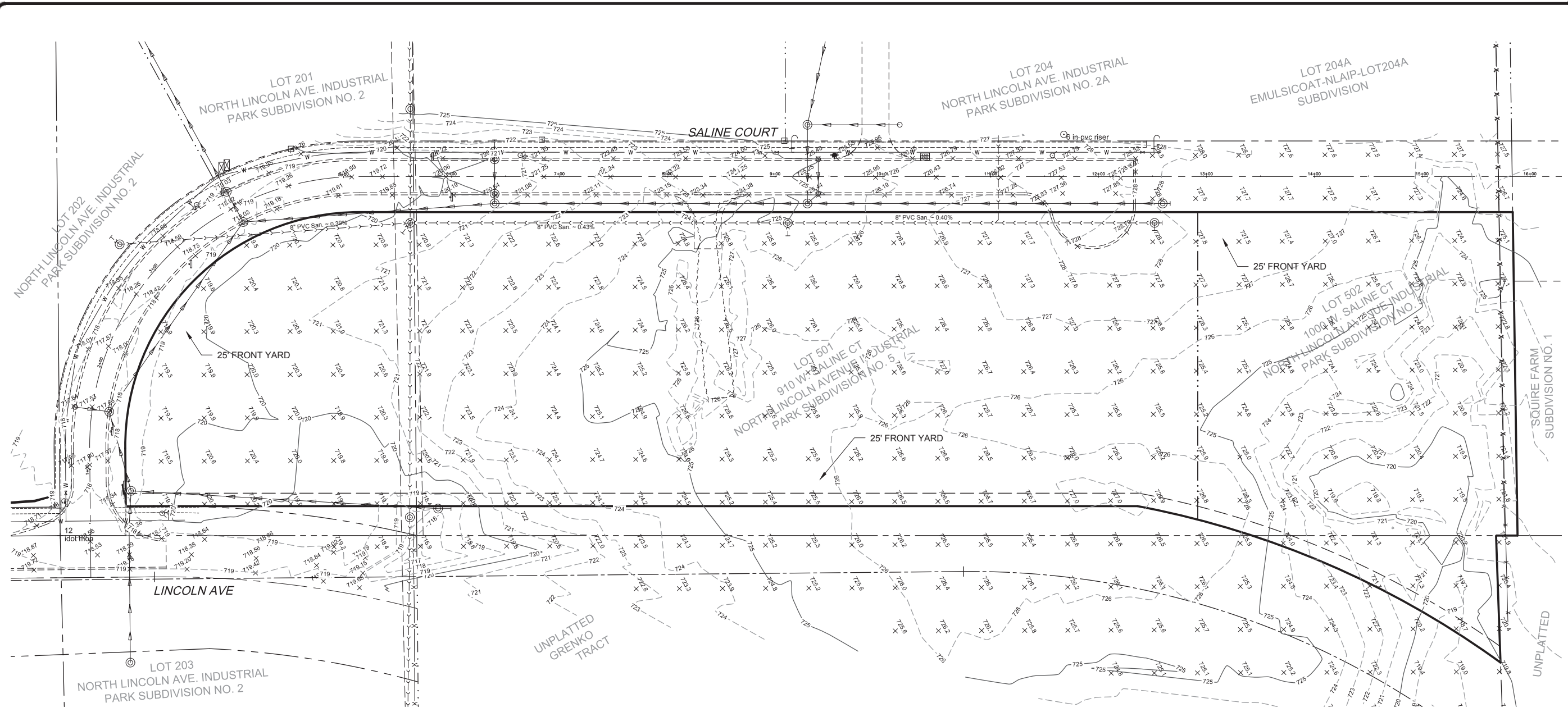
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GENERAL NOTES

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SHEET -1- OF -5-



- BENCHMARKS**
- BM 1 CHISELED SQUARE ON NW CORNER OF NORTH LINCOLN AVENUE BRIDGE NORTH OF WILBUR RD. ELEV. 719.41
 - BM 2 CHISELED SQUARE ON NW CORNER OF SOMER STREET BRIDGE OVER SALINE DITCH ELEV. 721.24
- 100 YR. H. W. ELEV. = 716.52
ON SALINE BRANCH DRAINAGE DITCH
- REFERENCE: BRIDGE PLANS, LINCOLN AVE. OVER SALINE BRANCH STRUCTURE NO. 010-6130

THIRD PARTY TOPOGRAPHIC SURVEY
INFORMATION PROVIDED BY THE OWNER

LEGEND

---	LOT LINE
- - -	CENTERLINE
---	RIGHT-OF-WAY LINE
---<---<---	EXIST. SANITARY SEWER
⊙	EXIST. MANHOLE
●	PROP. MANHOLE
⊙	FUTURE MANHOLE
- - - 714	EXIST. CONTOUR LINE
~>	PROP. DIRECTION OF FLOW
720.0 x	PROP. SPOT ELEVATION (ADD 700.00)
---	BUILDING SETBACK LINE

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**SOUTHWIND RAS
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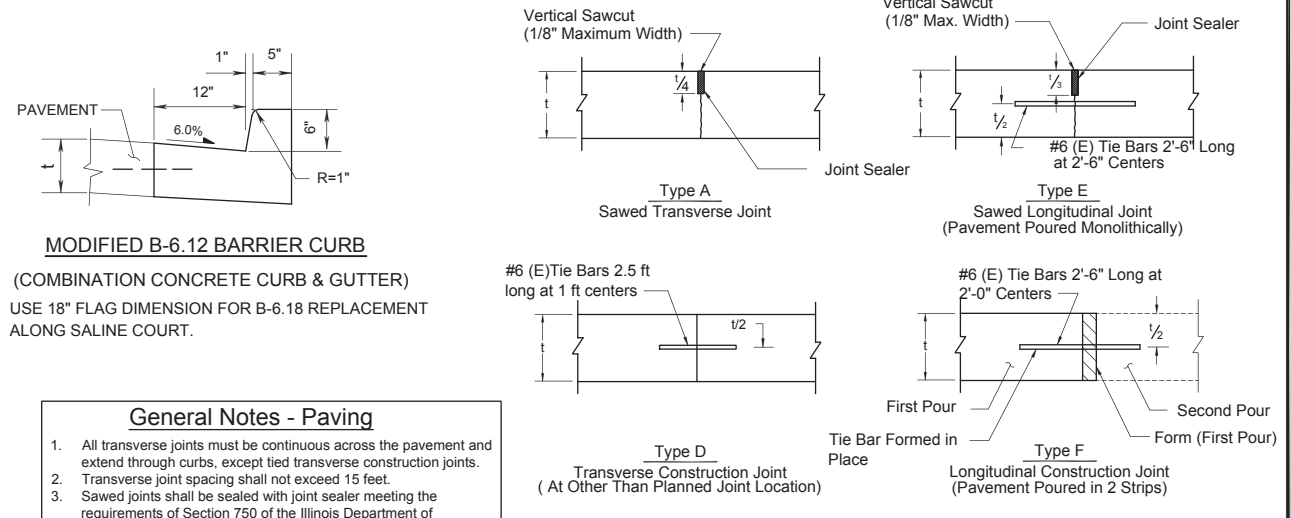
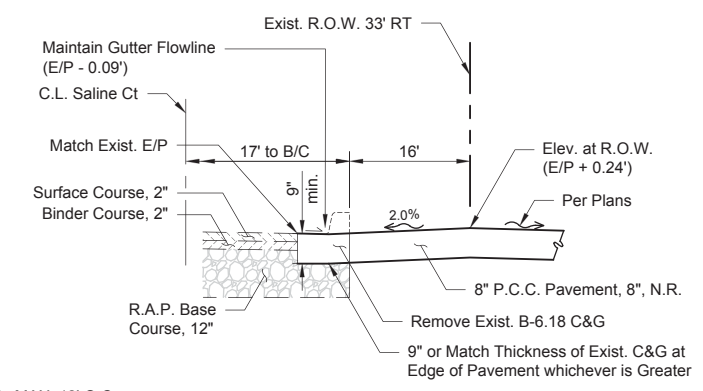
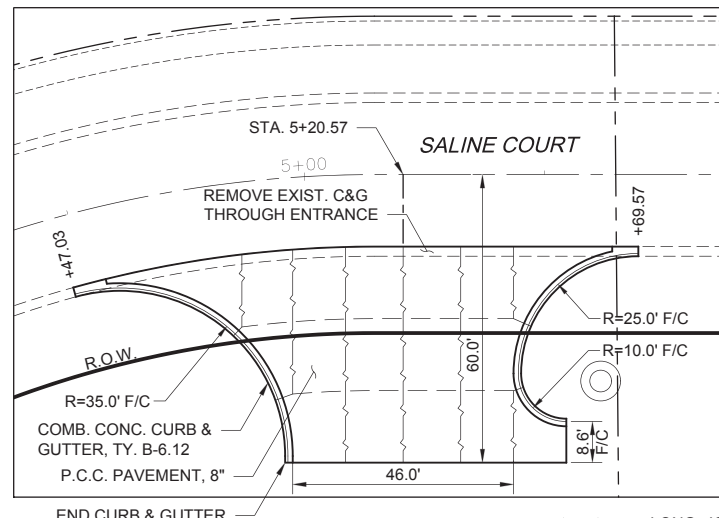
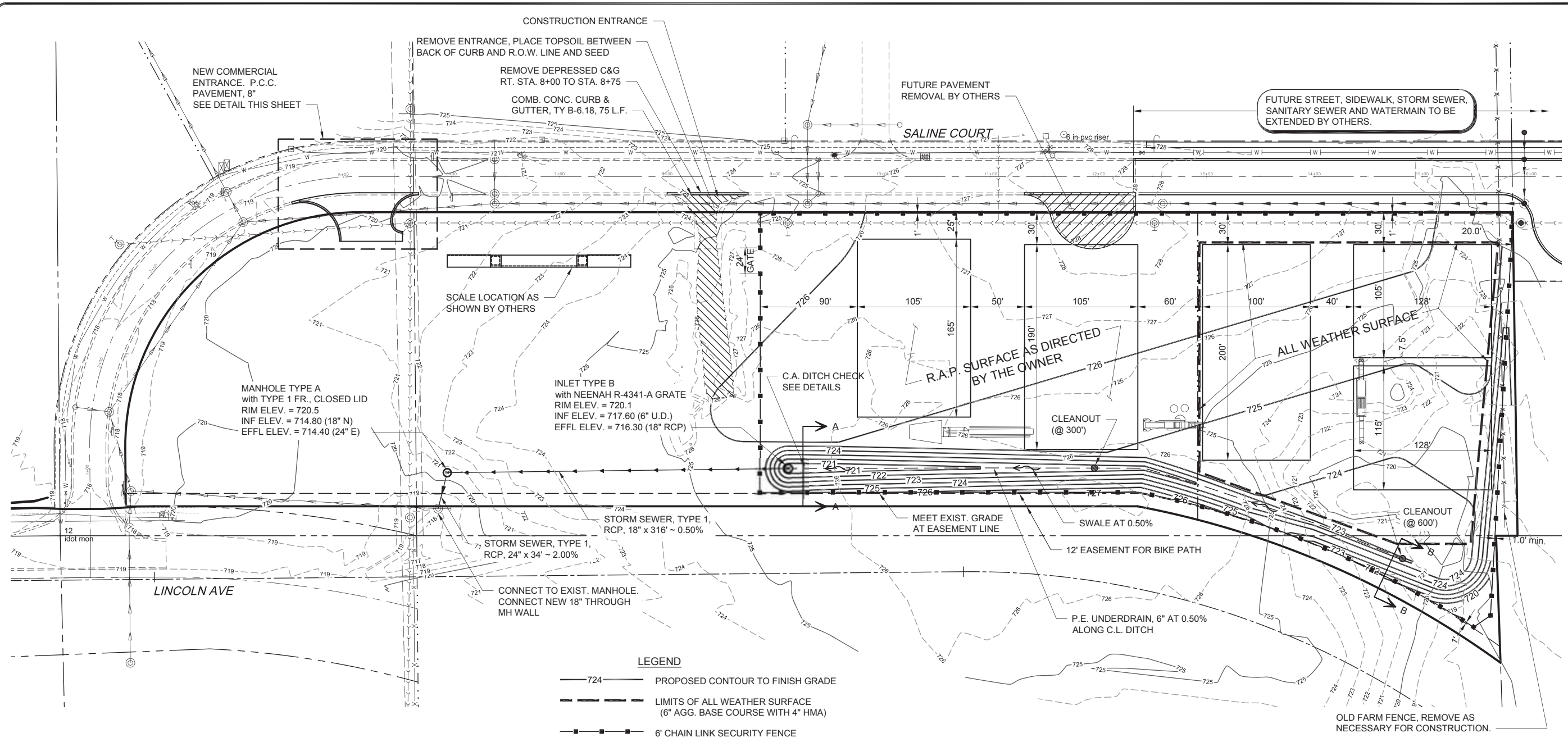
EXISTING TOPOGRAPHY

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SHEET -2- OF -5-

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- General Notes - Paving**
- All transverse joints must be continuous across the pavement and extend through curbs, except tied transverse construction joints.
 - Transverse joint spacing shall not exceed 15 feet.
 - Sawed joints shall be sealed with joint sealer meeting the requirements of Section 750 of the Illinois Department of Transportation "Standard Specifications for Road and Bridge Construction."
 - Joint Sealant: hot poured shall conform to ASTM D3405. Material shall be PAF-3; Cold Poured joint sealant not allowed. See Special Provisions for Additional Requirements.

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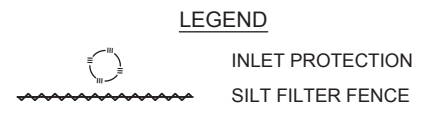
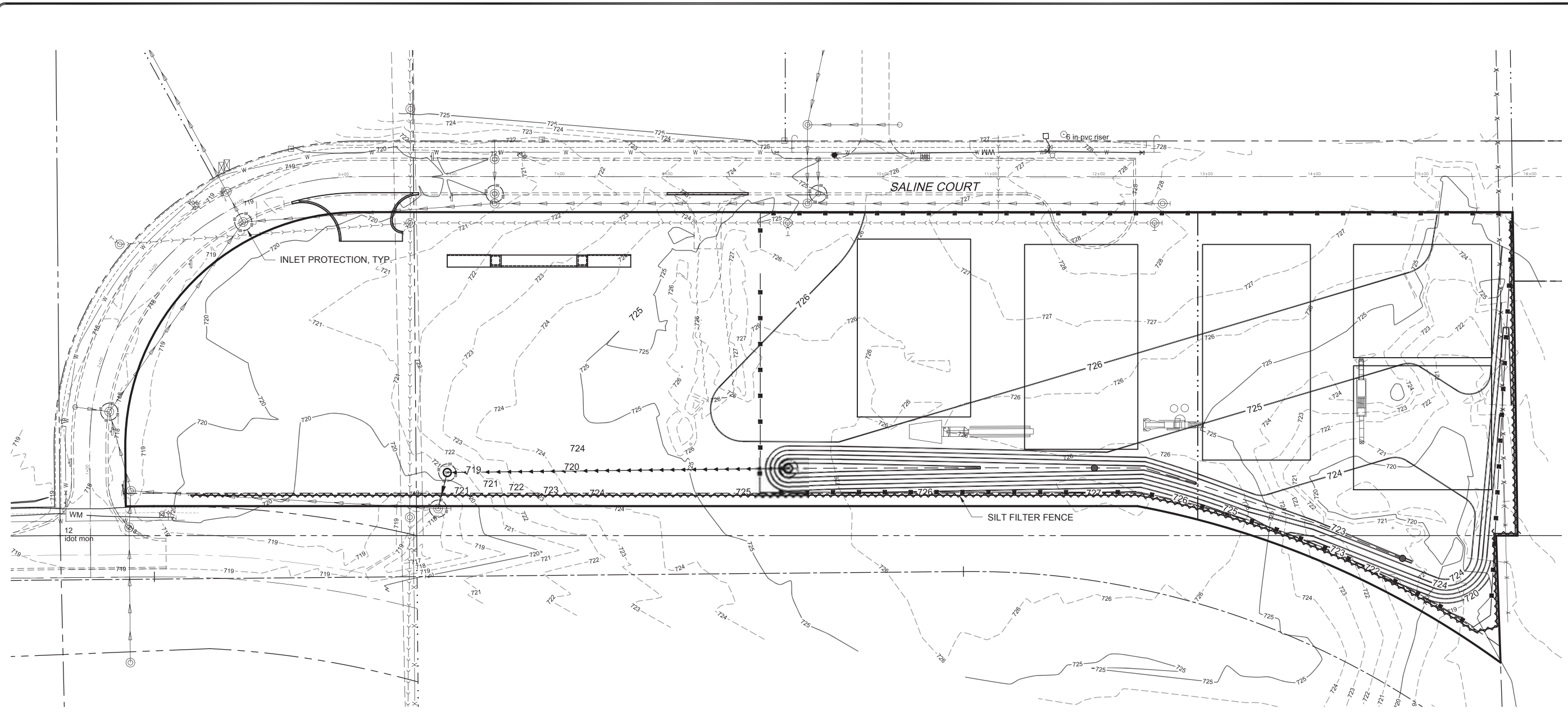
GRADING AND SITE PLAN

HORIZONTAL SCALE:
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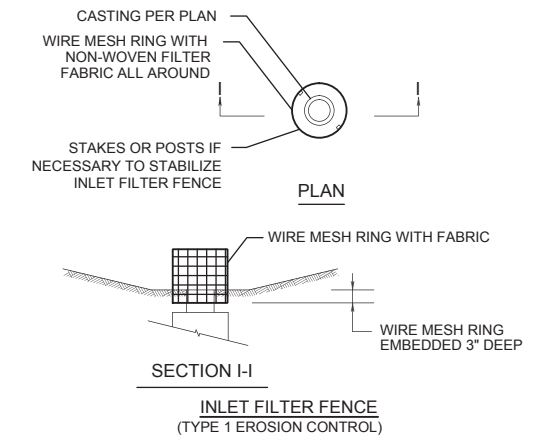
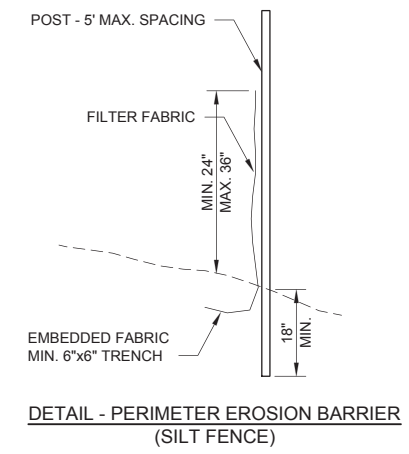
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SEE SHEET C-1 FOR GENERAL NOTES



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**SOUTHWIND RAS
PHASE 1 RECYCLED ASPHALT SHINGLES**

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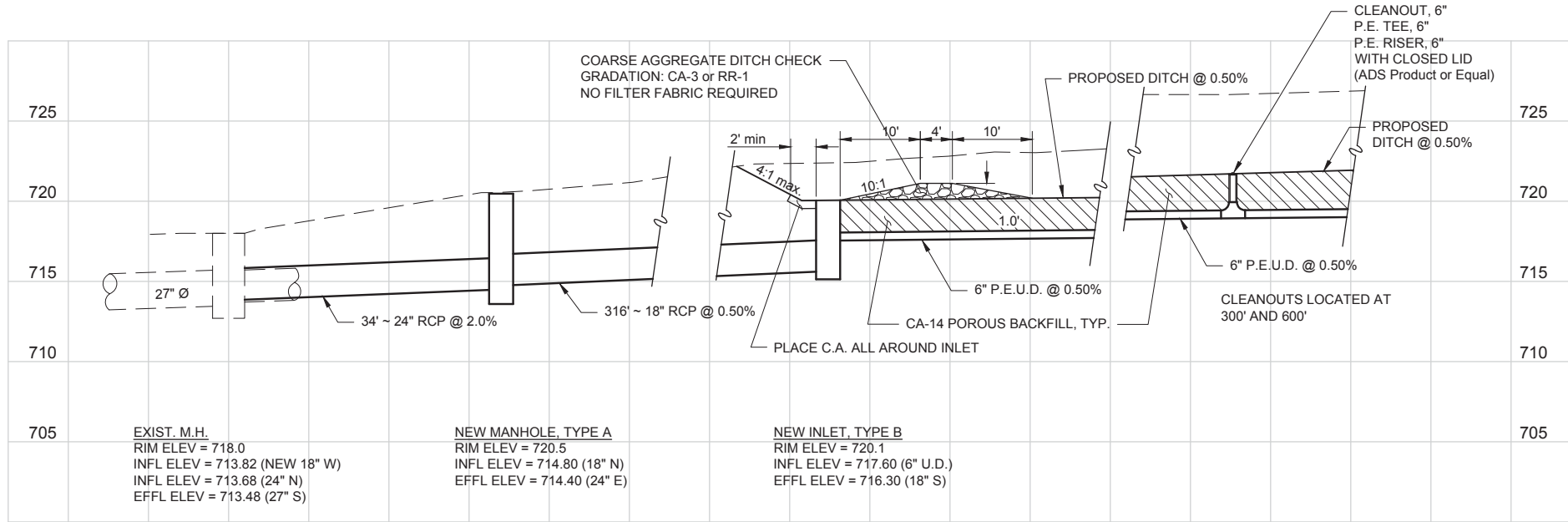
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**STORMWATER
POLLUTION
PREVENTION PLAN**

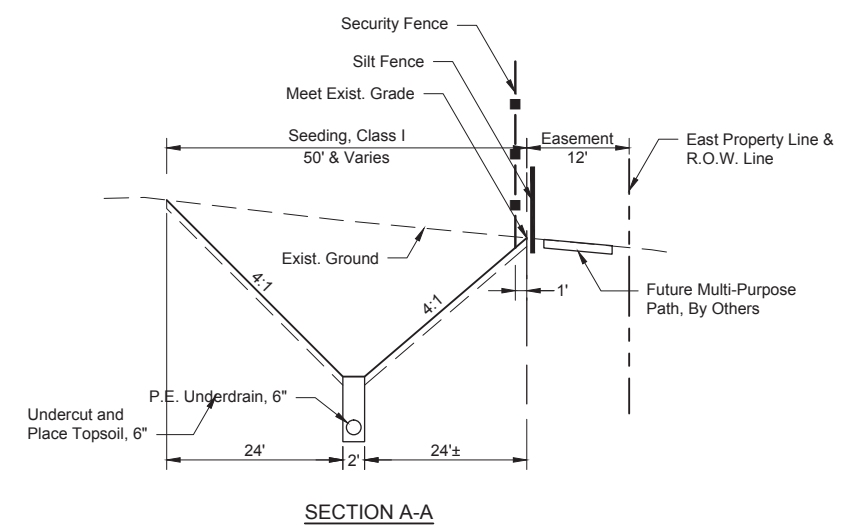
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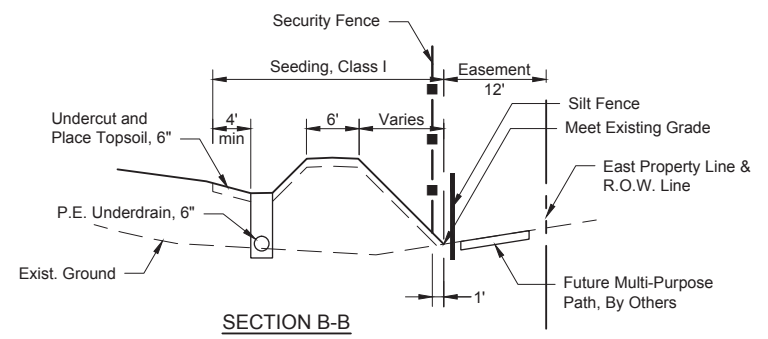
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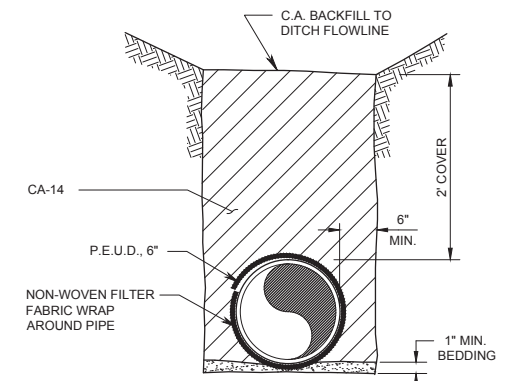
STORM SEWER PROFILE



SECTION A-A



SECTION B-B



PERFORATED PIPE UNDERDRAIN TRENCH DETAIL

PERFORATED, CORRUGATED P.E. PIPE WITH A SMOOTH INTERIOR CONFORMING TO ARTICLE 1040.19 OF THE STANDARD SPECIFICATIONS ("ADS N-12" OR EQUAL)

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DETAILS

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